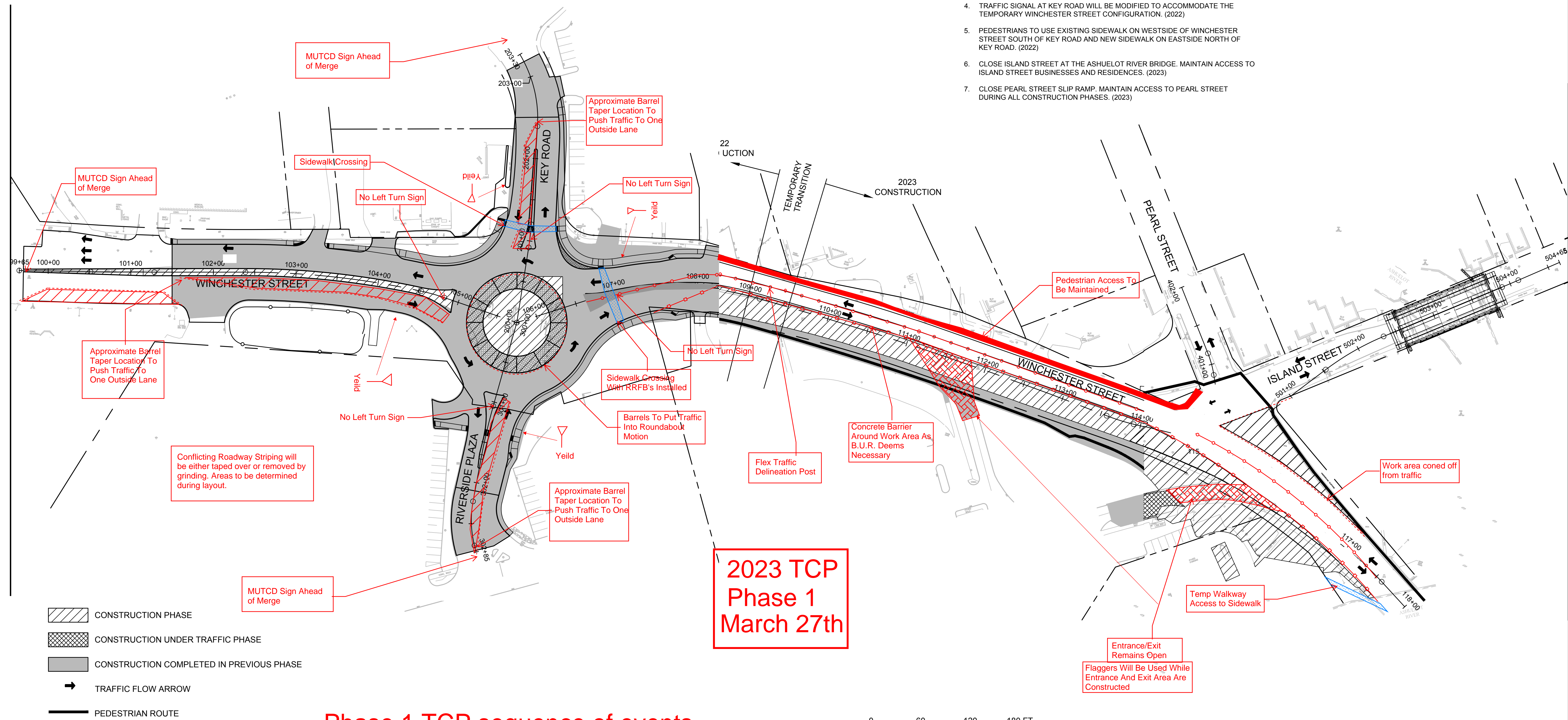


PHASE 2 CONSTRUCTION:

1. BEGIN ISLAND STREET BRIDGE DEMOLITION AND CONSTRUCTION. (2023)
2. INSTALL DRAINAGE CB'S AND PIPES ON EASTERN SIDE OF WINCHESTER STREET. CONNECT TO THOSE INSTALLED IN PHASE 1.
3. COMPLETE WATER AND SEWER UTILITY IMPROVEMENTS ALONG ISLAND AND PEARL STREETS. (2023)
4. PERMANENTLY GRADE AND PAVE EASTERN SIDE OF WINCHESTER STREET AND RIVERSIDE PLAZA DRIVEWAY.

PHASE 2 MAINTENANCE OF TRAFFIC:

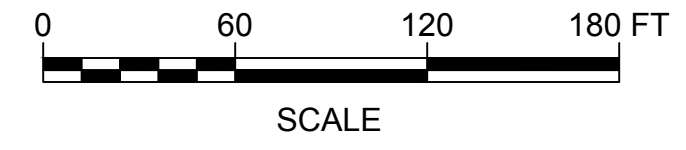
1. CLOSE THE EASTERN SIDE OF WINCHESTER STREET. MAINTAIN 1 LANE IN EACH DIRECTION ON THE WESTERN SIDE OF WINCHESTER STREET.
2. MAINTAIN THE INTERSECTION OF WINCHESTER STREET, KEY ROAD, AND RIVERSIDE PLAZA SO THAT LARGE DELIVERY TRUCKS WILL BE ABLE TO ACCESS BUSINESSES ON RIVERSIDE PLAZA AND KEY ROAD. (2022)
3. USE TAPERS AND MERGES TO ENSURE THE EXISTING ROUNDABOUT TO THE SOUTH AT THE INTERSECTION OF NH-10, NH-12, NH-101, AND WINCHESTER STREET CONTINUES TO OPERATE REGULARLY. (2022)
4. TRAFFIC SIGNAL AT KEY ROAD WILL BE MODIFIED TO ACCOMMODATE THE TEMPORARY WINCHESTER STREET CONFIGURATION. (2022)
5. PEDESTRIANS TO USE EXISTING SIDEWALK ON WESTSIDE OF WINCHESTER STREET SOUTH OF KEY ROAD AND NEW SIDEWALK ON EASTSIDE NORTH OF KEY ROAD. (2022)
6. CLOSE ISLAND STREET AT THE ASHUELOT RIVER BRIDGE. MAINTAIN ACCESS TO ISLAND STREET BUSINESSES AND RESIDENCES. (2023)
7. CLOSE PEARL STREET SLIP RAMP. MAINTAIN ACCESS TO PEARL STREET DURING ALL CONSTRUCTION PHASES. (2023)



- CONSTRUCTION PHASE
- CONSTRUCTION UNDER TRAFFIC PHASE
- CONSTRUCTION COMPLETED IN PREVIOUS PHASE
- TRAFFIC FLOW ARROW
- PEDESTRIAN ROUTE

Phase 1 TCP sequence of events

1. Subcontractor N.E.S.S. will install the 4 RRFB's on Winchester Street by Riverside Plaza roundabout to allow pedestrians to cross intersection when traffic is switched into roundabout motion. Traffic lights will remain in place until just before phase 3 TCP is activated.
2. Traffic on Winchester Street will be reduced to one outside lane in each direction coming into the Riverside Plaza roundabout. Traffic will be reduced to one lane by utilizing barrel tapers and cone divided lanes.
2. Traffic on Key Road and Riverside Plaza will be reduced to one outside lane coming into the Riverside Plaza roundabout. Traffic will be reduced to one lane by utilizing barrel tapers.
3. Barrels will be set out to form the outside of the roundabout.
4. Roundabout pavement will be removed and one way signs will be installed in each direction to alert traffic of new traffic pattern.
5. Flex post traffic delineation will be installed from Approximate Stationing 107+00 to 118+00.
6. Concrete median barrier will be removed and repositioned.



1	12/3/2021	ADJUST FOR PLANNED 2-YEAR CONSTRUCTION	
2	12/10/2021	MISSING HATCH PATTERNS RESTORED	
REV	DATE	DESCRIPTION	BY

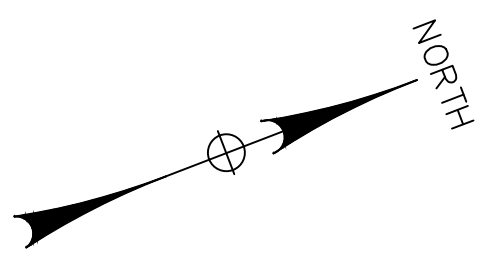
McFarland Johnson
53 REGIONAL DRIVE
CONCORD, NEW HAMPSHIRE 03301

**CITY OF KEENE
KEENE, NEW HAMPSHIRE
WINCHESTER STREET
RECONSTRUCTION**

**PHASING PLAN
(SHEET 2 OF 4)**

SCALE: 1"=60'	DESIGN: KJM	CS-02 71 OF 143
DRAWN: MJF	PROJECT: 18022.01	
CHECKED: EWM	DATE: SEPTEMBER 2021	

M:\18022.01 KEENE WINCHESTER FINAL DRAWINGS\SHSHEET FILES\18022.01-CSPP.DWG

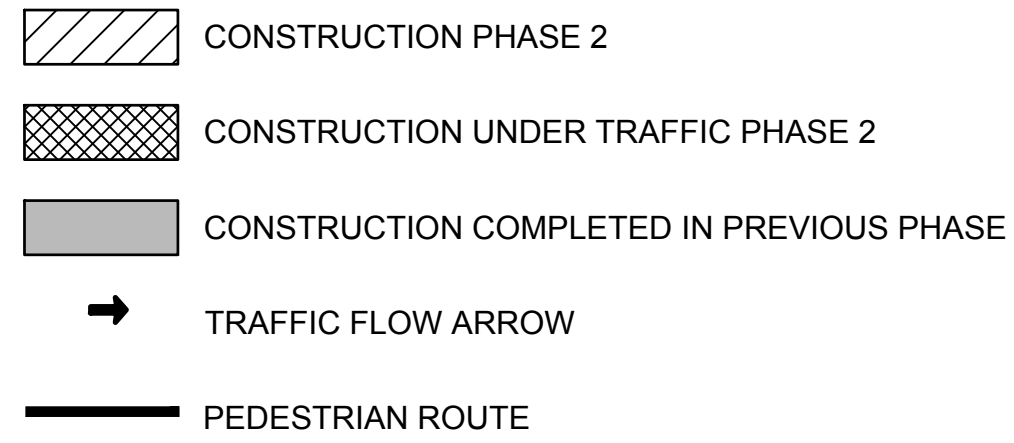
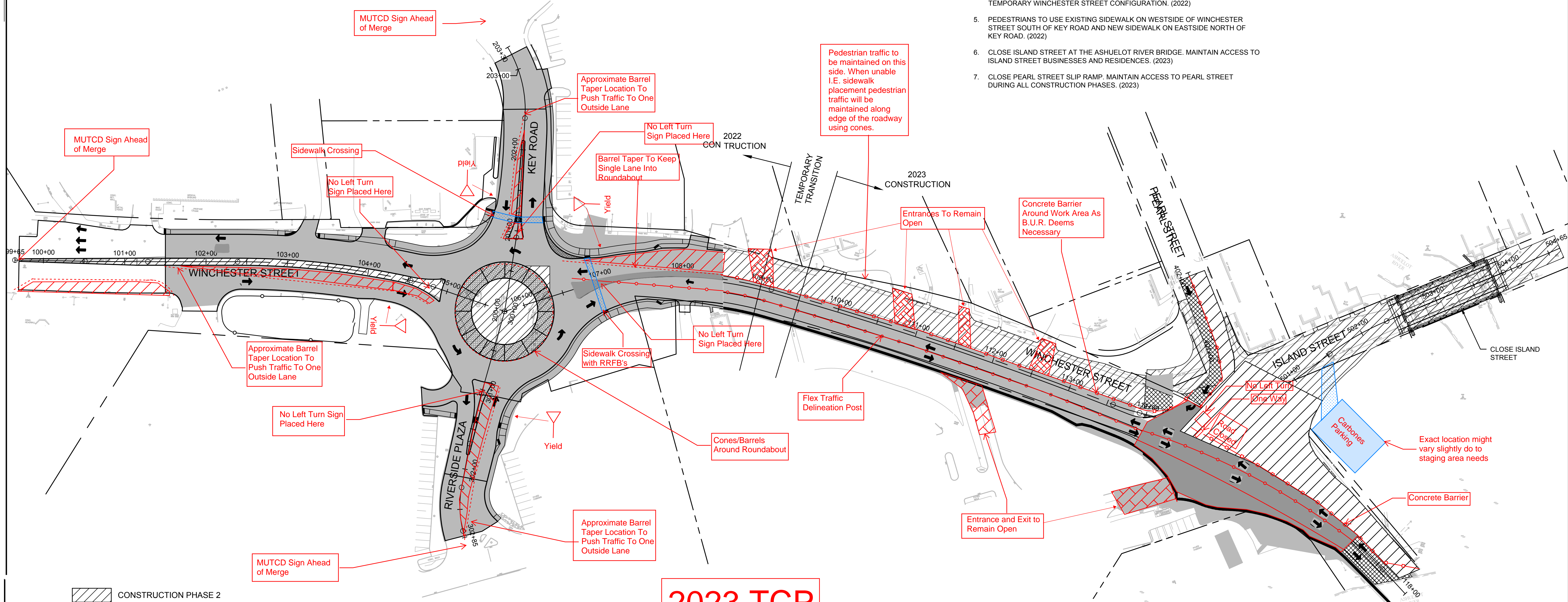


PHASE 2 CONSTRUCTION:

1. BEGIN ISLAND STREET BRIDGE DEMOLITION AND CONSTRUCTION. (2023)
2. INSTALL DRAINAGE CB'S AND PIPES ON EASTERN SIDE OF WINCHESTER STREET. CONNECT TO THOSE INSTALLED IN PHASE 1.
3. COMPLETE WATER AND SEWER UTILITY IMPROVEMENTS ALONG ISLAND AND PEARL STREETS. (2023)
4. PERMANENTLY GRADE AND PAVE EASTERN SIDE OF WINCHESTER STREET AND RIVERSIDE PLAZA DRIVEWAY.

PHASE 2 MAINTENANCE OF TRAFFIC:

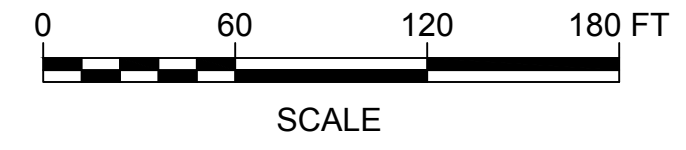
1. CLOSE THE EASTERN SIDE OF WINCHESTER STREET. MAINTAIN 1 LANE IN EACH DIRECTION ON THE WESTERN SIDE OF WINCHESTER STREET.
2. MAINTAIN THE INTERSECTION OF WINCHESTER STREET, KEY ROAD, AND RIVERSIDE PLAZA SO THAT LARGE DELIVERY TRUCKS WILL BE ABLE TO ACCESS BUSINESSES ON RIVERSIDE PLAZA AND KEY ROAD. (2022)
3. USE TAPERS AND MERGES TO ENSURE THE EXISTING ROUNDABOUT TO THE SOUTH AT THE INTERSECTION OF NH-10, NH-12, NH-101, AND WINCHESTER STREET CONTINUES TO OPERATE REGULARLY. (2022)
4. TRAFFIC SIGNAL AT KEY ROAD WILL BE MODIFIED TO ACCOMMODATE THE TEMPORARY WINCHESTER STREET CONFIGURATION. (2022)
5. PEDESTRIANS TO USE EXISTING SIDEWALK ON WESTSIDE OF WINCHESTER STREET SOUTH OF KEY ROAD AND NEW SIDEWALK ON EASTSIDE NORTH OF KEY ROAD. (2022)
6. CLOSE ISLAND STREET AT THE ASHUELOT RIVER BRIDGE. MAINTAIN ACCESS TO ISLAND STREET BUSINESSES AND RESIDENCES. (2023)
7. CLOSE PEARL STREET SLIP RAMP. MAINTAIN ACCESS TO PEARL STREET DURING ALL CONSTRUCTION PHASES. (2023)



B.U.R. Plans To Use One Lane To Construct Roundabouts And The Median Curb Islands. Traffic Will Be Pushed To The Available Opposite Lane During This Construction. Traffic Will Be Reset To Depicted Phase 3 Condition At The End Of Each Day.

***See Attachment For Island and Pearl Street Detour**

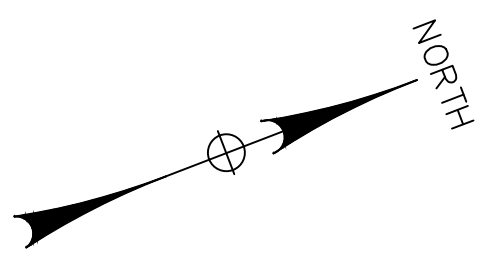
2023 TCP Phase 2 April 24th



1	12/3/2021	ADJUST FOR PLANNED 2-YEAR CONSTRUCTION	
2	12/10/2021	MISSING HATCH PATTERNS RESTORED	
REV	DATE	DESCRIPTION	BY
McFarland Johnson 53 REGIONAL DRIVE CONCORD, NEW HAMPSHIRE 03301			

CITY OF KEENE KEENE, NEW HAMPSHIRE WINCHESTER STREET RECONSTRUCTION PHASING PLAN (SHEET 2 OF 4)			
SCALE: 1"=60'	DESIGN: KJM	CS-02 71 OF 143	
DRAWN: MJF	PROJECT: 18022.01		
CHECKED: EWM	DATE: SEPTEMBER 2021		

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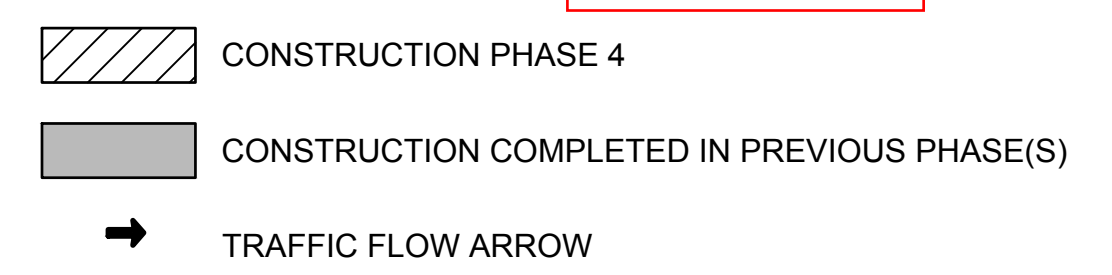
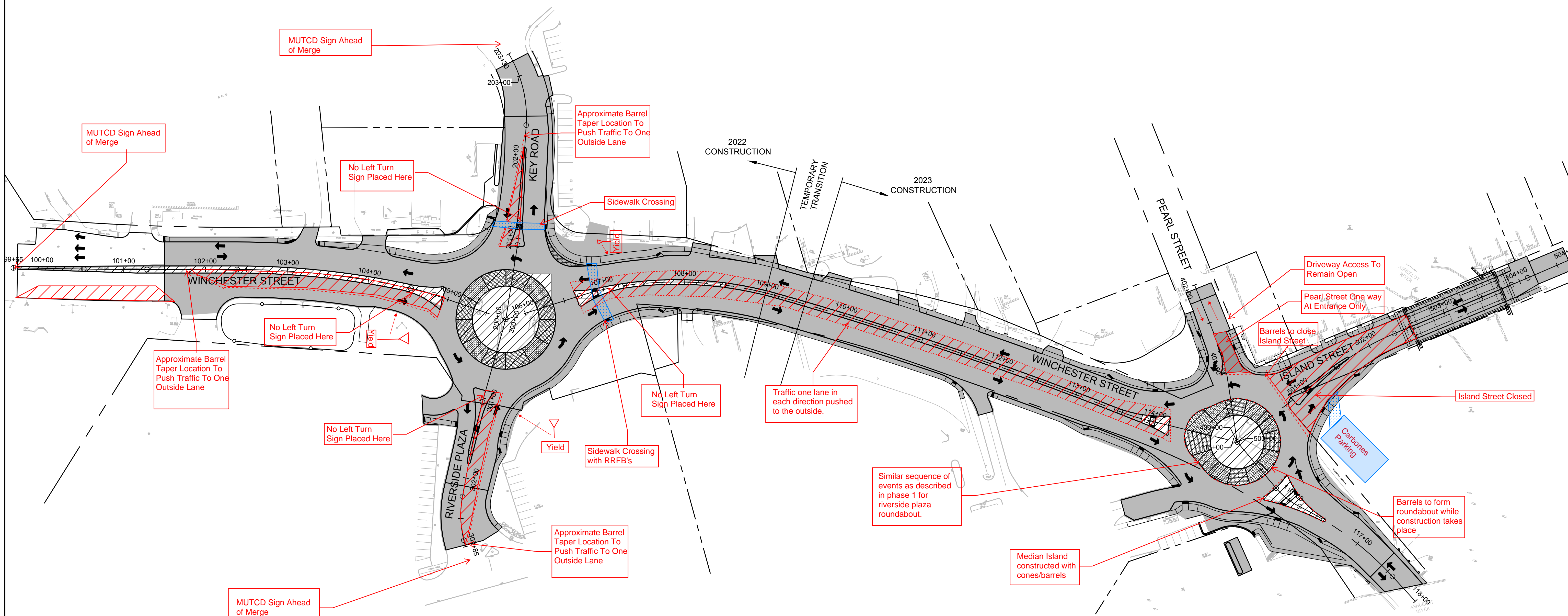


PHASE 4 CONSTRUCTION:

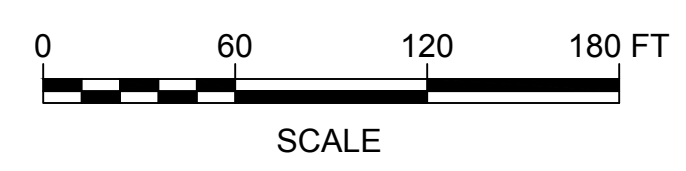
1. COMPLETE THE ROUNDABOUT APRONS AND ISLANDS.
2. COMPLETE ALL ISLANDS.
3. COMPLETE ANY CURBING THAT HAS NOT YET BEEN COMPLETED.
4. INSTALL WEARING COURSE PAVEMENT.
5. INSTALL PAVEMENT STRIPING.

PHASE 4 MAINTENANCE OF TRAFFIC:

1. UTILIZE BOTH ROUNDABOUTS AS SINGLE LANE ROUNDABOUTS UNTIL TRUCK APRONS ARE COMPLETED. ONCE THESE ARE COMPLETED TWO LANES MAY BE OPENED AROUND EACH ROUNDABOUT AS WELL AS ON EACH SIDE OF WINCHESTER STREET.
2. USE TAPERS AND MERGES TO ENSURE THE EXISTING ROUNDABOUT TO THE SOUTH AT THE INTERSECTION OF NH-10, NH-12, NH-101, AND WINCHESTER STREET CONTINUES TO OPERATE REGULARLY.
3. PEDESTRIANS TO USE NEW SIDEWALKS ON WINCHESTER STREET.



PHASE 3
July 24th
2023



1	12/3/2021	ADJUST FOR PLANNED 2-YEAR CONSTRUCTION	
2	12/10/2021	MISSING HATCH PATTERNS RESTORED	
REV	DATE	DESCRIPTION	BY

McFarland Johnson
 53 REGIONAL DRIVE
 CONCORD, NEW HAMPSHIRE 03301

CITY OF KEENE KEENE, NEW HAMPSHIRE WINCHESTER STREET RECONSTRUCTION	
PHASING PLAN (SHEET 4 OF 4)	
SCALE: 1"=60'	DESIGN: BEP
DRAWN: MJF	PROJECT: 18022.01
CHECKED: EWM	DATE: SEPTEMBER 2021
CS-04 73 OF 143	

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